

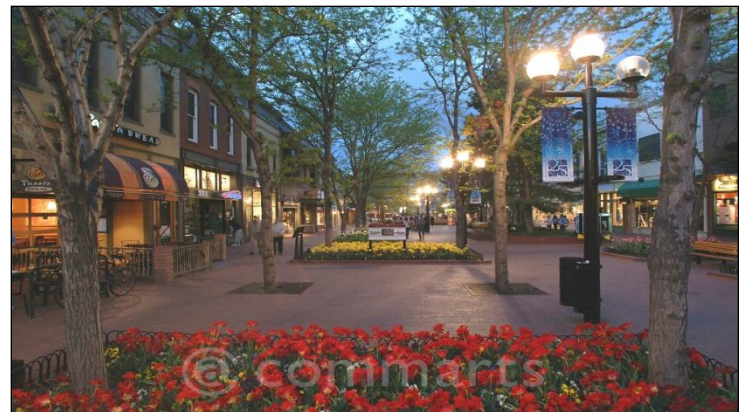
Parking Pricing and Management Boulder, Colorado

Kurt Matthews, Manager Parking
Services



Boulder, Colorado

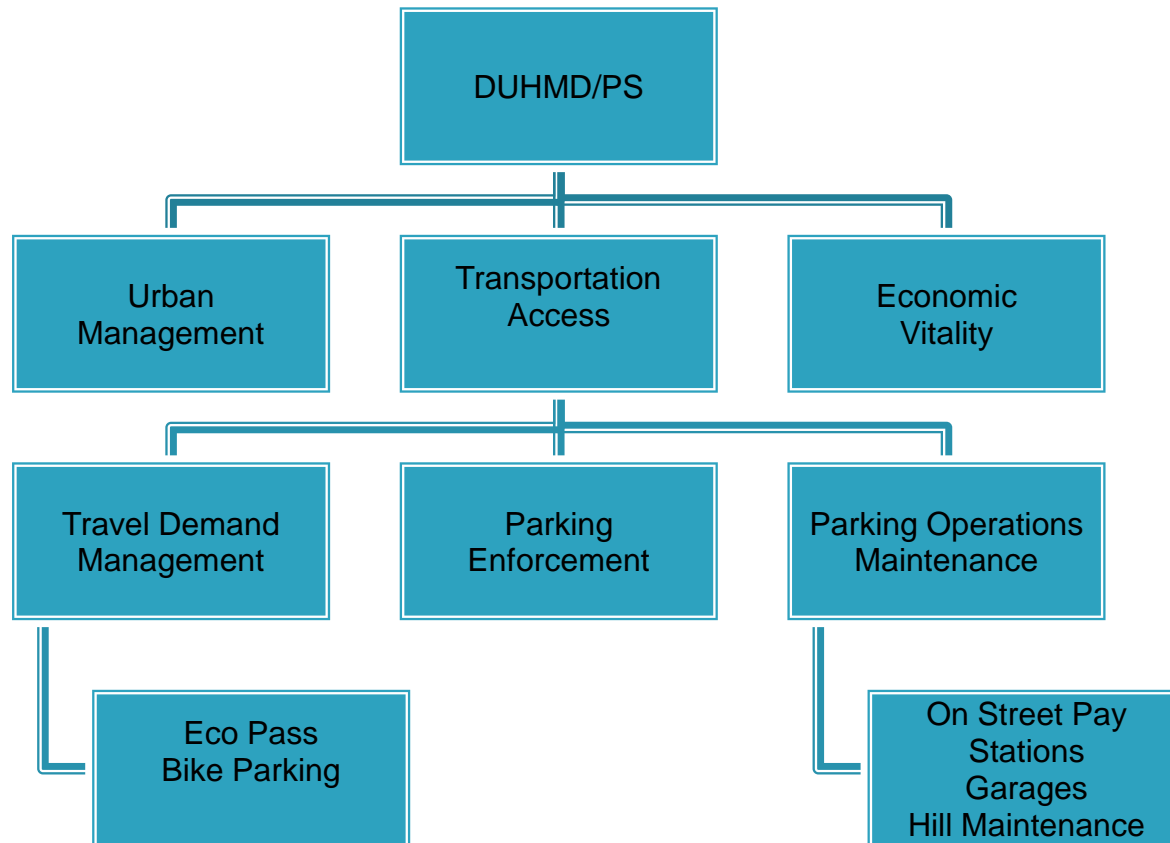
- ▶ 30 miles NW of Denver
- ▶ 100,000 pop.
- ▶ University of Colorado



Parking By the Numbers.....

- ▶ Parking Garages: 5 – 2,209 spaces
- ▶ On-street meters: 165 pay stations
- ▶ Surface lots: 4
- ▶ Neighborhood Zones: 10 – 2,500 permits
- ▶ Parking Tickets: 100,000/annually
- ▶ Annual budget: \$9.5 million

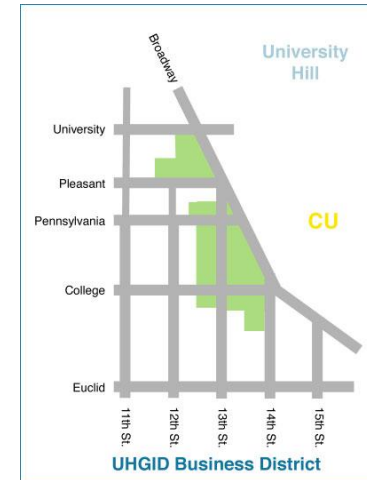
Tool #1 – Integration



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City of Boulder, Colorado

Tool #2: Parking District Concept

- ▶ Created in the 1970's:
- ▶ Overlay property tax district
- ▶ District provided parking
- ▶ No parking requirement
- ▶ Historic preservation
- ▶ Bonding capacity



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Tool #3: Managed Parking

- ▶ On Street
 - Paid parking
 - Coordinated with the garages
- ▶ Neighborhood Parking Program
 - Overflow onto the neighborhoods
- ▶ Garages:
 - Unbundled, Shared spaces
 - Both long and short term

Neighborhood Parking Programs

- ▶ NPP – created by ordinance
 - Three types of Permit:
 - Resident
 - Commuter
 - Business Employee

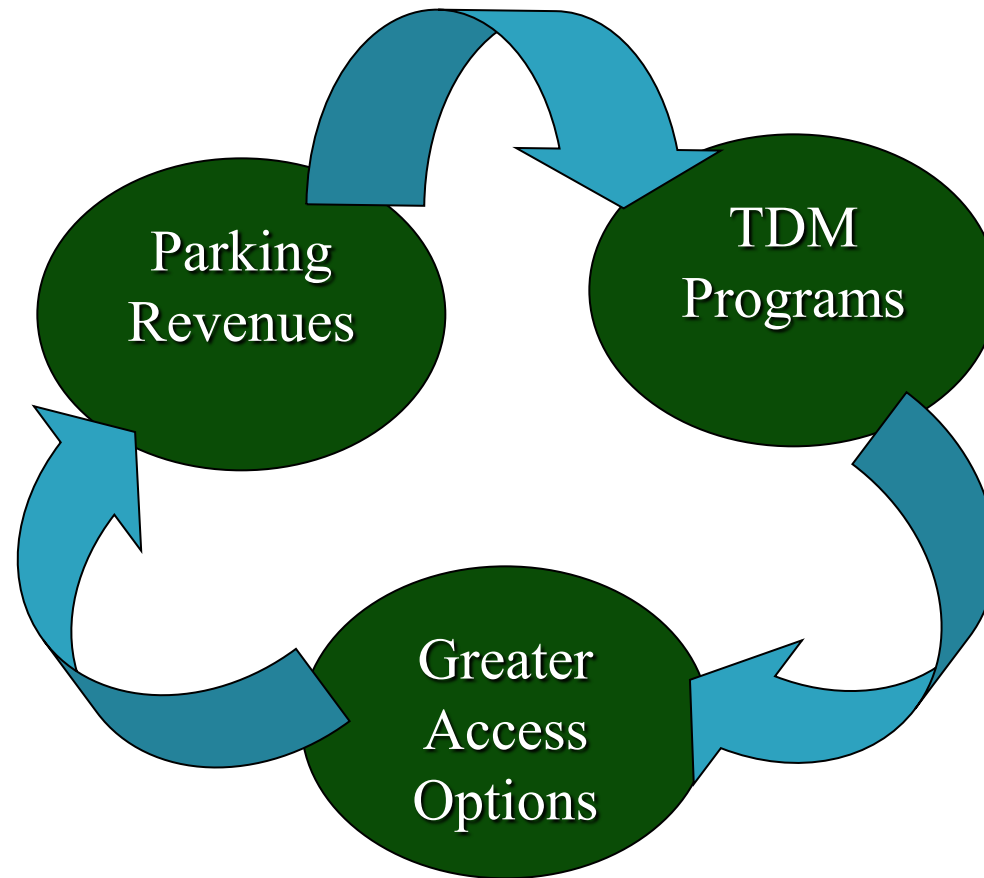
Parking Pricing – Short Term

- ▶ Parking Garages
 - \$1.25 per hour – first 4 hours
 - \$2.50 per hour after 4 hours
- ▶ On street
 - \$1.25 per hour – Maximums vary 2 hr–10 hr
 - Paystations – ticket valid anywhere in zone

Permit Pricing

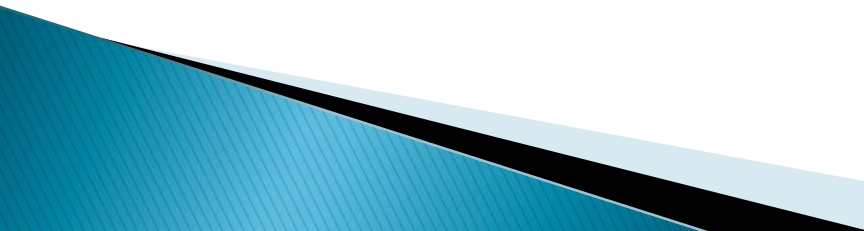
- ▶ Garages – permits sold quarterly – \$265/qtr
- ▶ Neighborhood Parking Permits
 - Resident – \$17/ vehicle per year – Limit 2
 - Commuter – \$78/ vehicle per quarter
 - Business Employee – \$75 / vehicle per quarter
- ▶ Surface Lots
 - Downtown lots – \$163 per quarter
 - Hill lots – \$155 per quarter

Parking Revenues Reinvestment

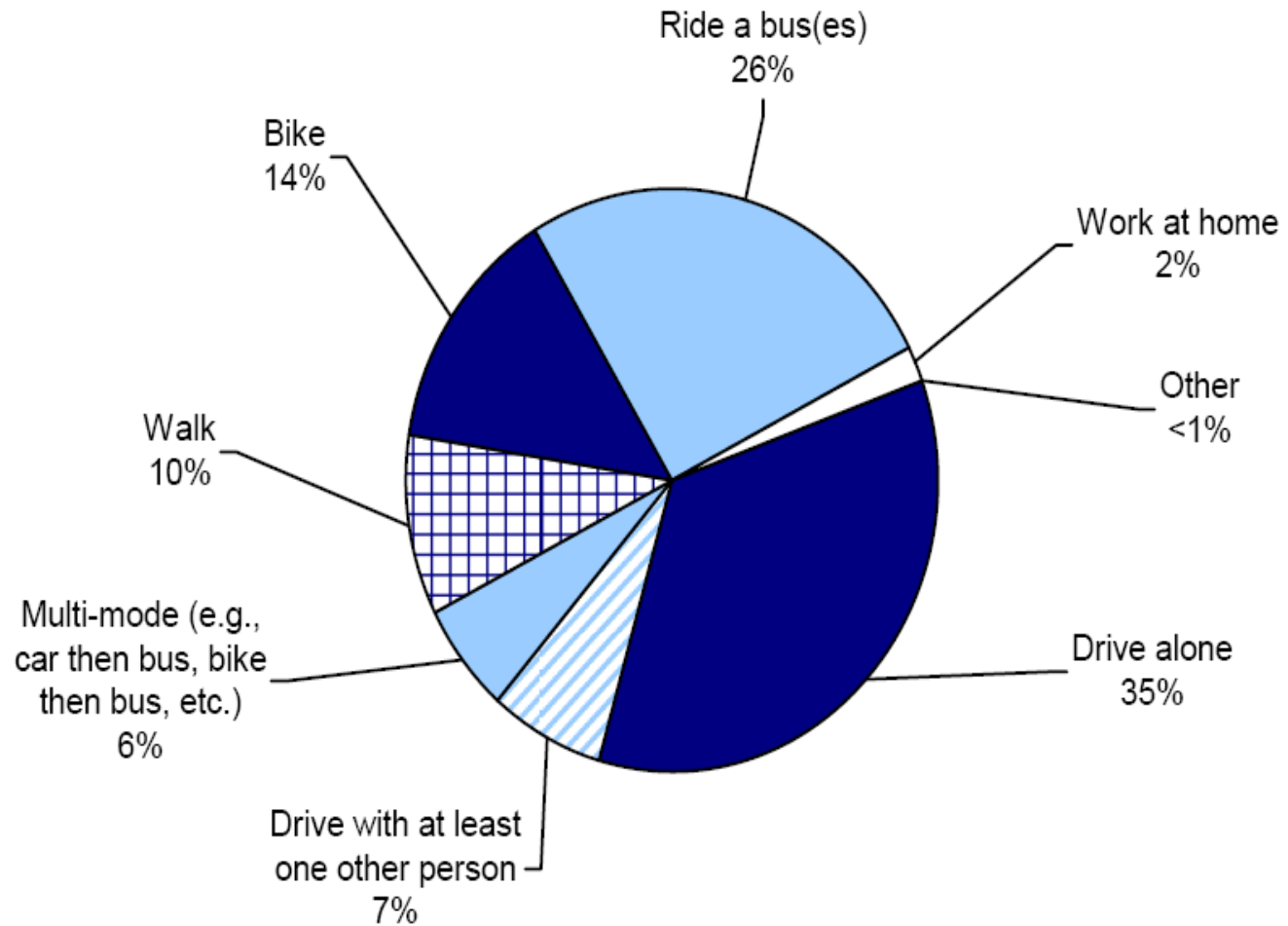


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City of Boulder, Colorado

TDM and Parking Management in Downtown Boulder

- ▶ Managing almost 4000 parking spaces
 - ▶ Providing Eco Passes
 - Approximately \$125 per employee in 2011
 - ▶ Eco Pass Extra Program
 - Merchant discounts
 - ▶ Paid on-street parking
 - ▶ Shared structured parking
 - ▶ Over 1,300 bicycle parking spaces provided
 - ▶ Enhanced Community Transit Network
 - ▶ Boulder B-Cycle recently launched
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2008 Downtown Employee Mode Split



What is an Eco Pass?

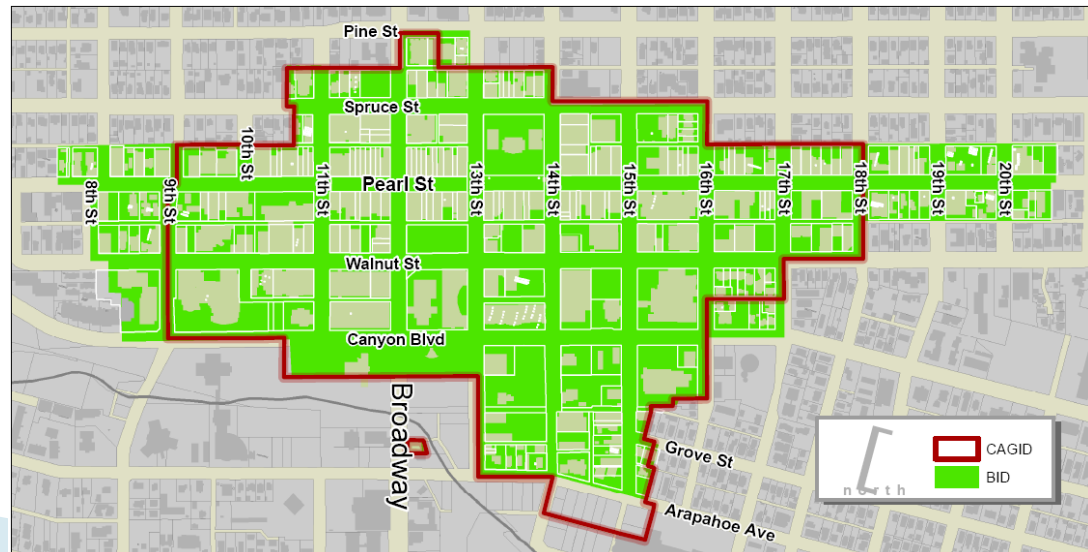
- ▶ Unlimited RTD bus/rail annual pass purchased for ALL employees
- ▶ Insurance model – purchase an EcoPass for every employee, not just those who request a bus pass
- ▶ Master contract agent where government agency acts on behalf of 1,200+ employers

Downtown Boulder Eco Pass Program

- ▶ **Central Area General Improvement District (CAGID)**
 - Established in 1970 in Downtown Boulder
 - Properties are taxed to provide shared structured parking and parking management services
 - Manage paid on-street parking

Paid Parking in CAGID

\$746K in revenues from paid parking were used to buy Eco Passes for downtown employees in 2011



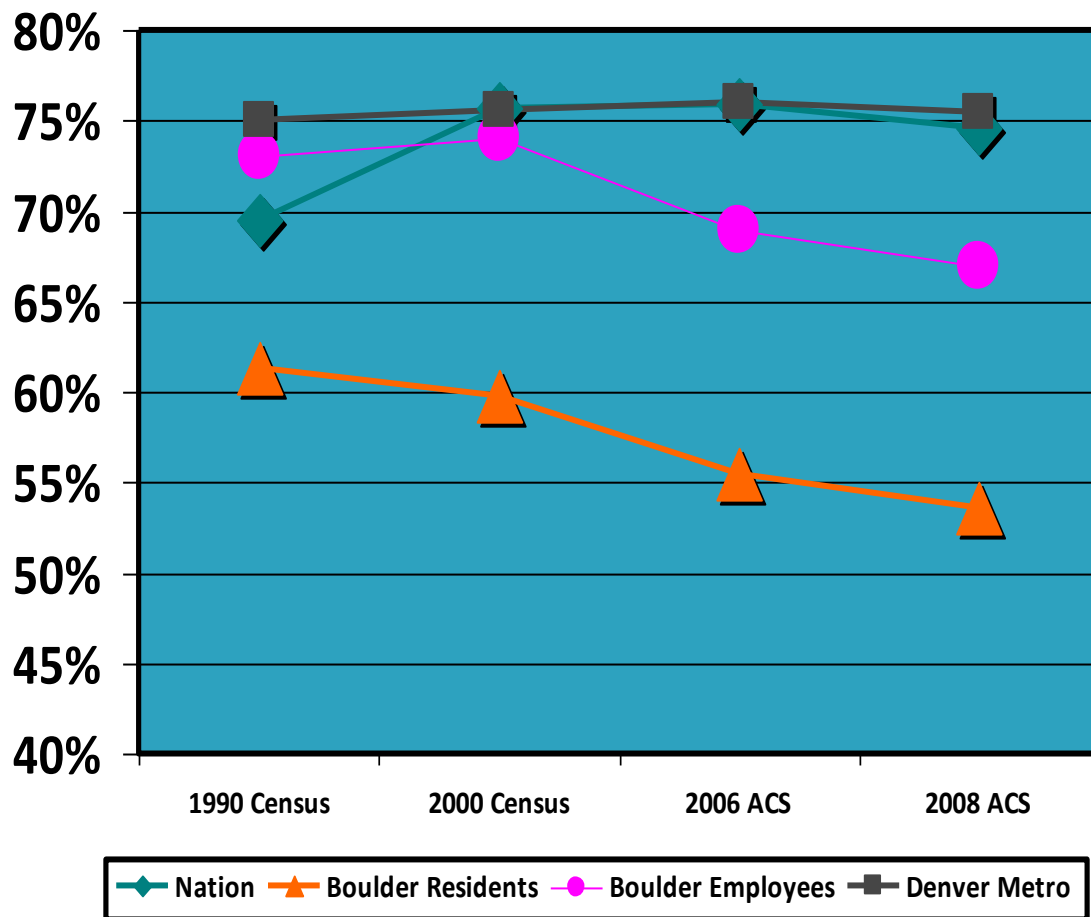
Downtown Employee Travel Behavior

- ▶ Downtown employees with Eco Passes:
 - Over three times more likely to ride transit
 - Twice as likely to bicycle
 - Three times more likely to walk
- ▶ Downtown employees with Eco Passes emit about 1mtCO₂ less per year than commuters without an Eco Pass



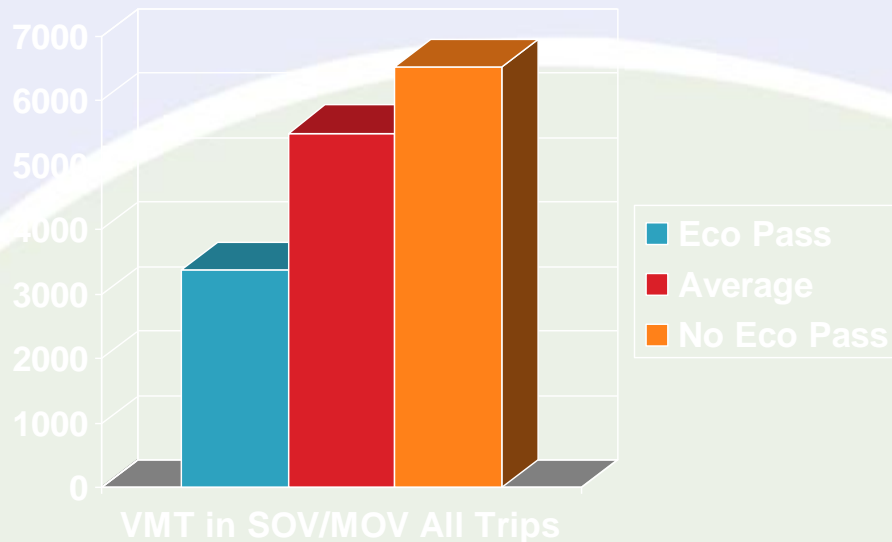
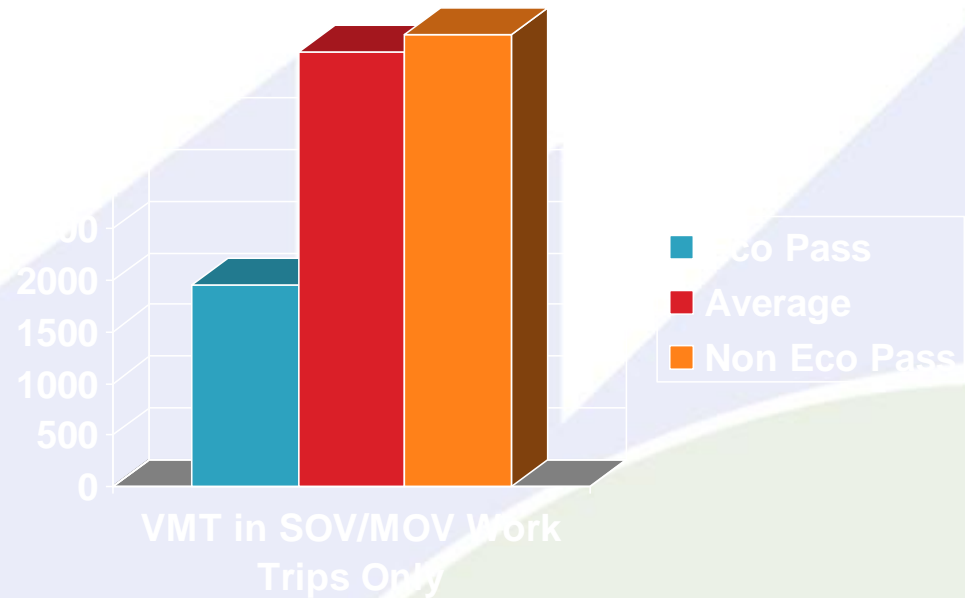
Drive Alone Work Trip Mode Share

Since 1990, the SOV mode share for work trip by Boulder residents and employees has declined significantly compared to the Denver Region and the nation.



Impact on Travel Behavior

- ▶ Boulder residents with an Eco Pass drive about **2,600 miles less** per year than residents without an Eco Pass.



- Boulder employees with an Eco Pass drive about **2,300 miles less** per year than employees without an Eco Pass.

Decline in SOV Trips

Since 1990, SOV trips by Boulder residents has declined by 7.1 percent, while bus and bike use has increased by almost 11 percent combined.

TMP 2025 goal is to reduce all trips to 25 percent SOV.

Travel Mode	Percent of Trips*									Change 1990 to 2009
	2009	2006	2003	2000	1998	1996	1994	1992	1990	
Single-Occupancy Vehicle	37.1%	38.4%	39.0%	41.5%	40.4%	41.5%	40.5%	42.3%	44.2%	-7.1%
Multiple-Occupancy Vehicle	23.7%	25.0%	23.5%	23.8%	25.0%	25.6%	25.6%	25.7%	26.3%	-2.6%
Transit	5.4%	4.0%	4.6%	4.2%	4.1%	2.8%	2.9%	2.2%	1.6%	+3.8%
School Bus	0.1%	0.1%	0.3%	0.7%	0.7%	0.5%	0.5%	0.7%	0.6%	-0.5%
Bicycle	15.9%	13.6%	14.0%	10.0%	8.2%	9.2%	11.3%	12.1%	9.1%	+6.8%
Foot	17.9%	18.9%	18.6%	19.8%	21.4%	20.4%	19.2%	17.1%	18.2%	-0.3%

Source: 2009 Travel Diary Data

Eco Passes and Work Trips

- ▶ Drive alone mode share with Eco Pass= 40.1%
- ▶ Drive alone mode share without Eco Pass= 74.6%
- ▶ Eco Pass holding employees are over 10 times more likely to use transit to get work in Boulder.

How did you get to work today?	Do you have an Eco-Pass? [*]	
	yes	no
Drove alone	40.1%	74.6%
Drove with at least one other person	6.5%	9.2%
Walked	7.0%	2.4%
Biked	6.0%	3.4%
Rode a bus or buses	32.3%	3.0%
Multi-mode	6.7%	2.6%
Worked at home	.9%	3.8%
Other	.5%	1.0%
Total	100.0%	100.0%

^{*} Differences between at least two subgroups are statistically significant, $p < 0.05$, tested with chi-square.

TDM and ECO Pass

- ▶ Eco Pass... a triple benefit
 - Employee benefit: retention/recruiting
 - Reducing costs - fewer spaces, less maintenance costs
 - Less congestion & reduction in greenhouse gasses

Eco Pass Value:

\$125/year/employee

Parking space costs:

\$24,000/construct

\$600/year to maintain



TDM: Bike Parking

- ▶ Funded by auto parking revenues
- ▶ Bike parking spaces on street and in garages
- ▶ Adding bike corral on street

Applying Lessons Learned

- ▶ **Boulder Junction TOD**
 - **TDM and Parking Management Access District**
 - Trip Generation Allowance
 - Resident and Employee Eco Passes
 - Parking maximums
 - Shared parking
 - Unbundled parking
 - Increased density



Challenges

- ▶ Maintaining the theory and practice of shared, managed parking in the district.
 - Parking supply often linked to financing for new developments or redevelopments.
 - Certain employers and developers expect a supply of private parking on site.

Questions?

Thank-you